



# Public Notice

U.S. Army Corps  
of Engineers  
Pittsburgh District

In Reply Refer to  
Notice No. below

US Army Corps of Engineers, Pittsburgh District  
1000 Liberty Avenue  
Pittsburgh, PA 15222-4186

Application No. 199701735

Date: November 7, 2005

Notice No. 05-67

Closing Date: December 6, 2005

1. TO ALL WHOM IT MAY CONCERN: The following application has been submitted for a Department of the Army Permit under the provisions of Section 404 of the Clean Water Act (33 U.S.C. 1344).

2. APPLICANT: PennDOT District 2-0  
1924-30 Daisy Street  
P.O. Box 42  
Clearfield, PA 16830

3. LOCATION: across the Clarion River and in adjacent wetlands, the West Branch Clarion River and Johnson Run and in adjacent wetlands in Johnsonburg, Elk County, Pennsylvania.

4. PURPOSE AND DESCRIPTION OF WORK: The applicant is proposing to construct two sections, C10 and D10, of a 1.5 miles roadway bypass along State Route 0219 around the Borough of Johnsonburg. Currently, S.R. 0219 runs through the center of operations of a paper mill. This bypass will improve safety and traffic flow and will separate local traffic and heavy truck traffic associated with the paper mill. The selected alternative is a result of extensive coordination between PennDOT, Citizen's Advisory Committee, local officials, the paper mill staff, the general public, and environmental resources agencies to identify an acceptable alignment that would address safety and traffic issues with minimal environmental impacts. The proposed two and three lane bi-directional bypass will begin on S.R. 0219 just north of S.R. 0255 and proceed northwest across the Allegheny and Eastern railroad line, floodplains and the Clarion River. After the Clarion River crossing, it will run parallel to the existing Buffalo and Pittsburgh railroad line to an at-grade intersection with Grant Street. From Grant Street, the alignment will continue northward and cross over the Buffalo and Pittsburgh Railroad and an associated culvert conveying Johnson Run beneath the railroad. The alignment then proceeds in a northern direction over West Center Street, the Allegheny and Eastern Railroad, floodplains and the West Branch of the Clarion River. The alignment then proceeds in a northeastern direction with at-grade intersections at Fairview Avenue and S.R. 4007 (West Main Street) to the northern termini on existing S.R. 0219 adjacent to

the Johnsonburg Jubilee Market Plaza. The project will permanently impact 1.01 acre of palustrine emergent wetlands and temporarily impact 0.04 acre of palustrine emergent wetland in Section C10. There are no wetland impacts in D10. Piers will permanently impact 0.01 acre of the West Branch Clarion River in Section D10. Wetland mitigation will consist of constructing 1.09 acres of replacement wetlands in the floodplain of the Clarion River. Drawings of the proposed project are attached to this notice.

5. ENCROACHMENT PERMIT: The applicant is required to obtain an encroachment permit which includes State 401 Water Quality Certification from the:

Pennsylvania Department of Environmental Protection  
Northwest Regional Office  
Soils and Waterways Section  
230 Chestnut Street  
Meadville, Pennsylvania 16335

Telephone: 814-332-6942

6. IMPACT ON NATURAL RESOURCES: The District Engineer has consulted the most recently available information and has determined that the project is not likely to affect the continued existence of any endangered species or threatened species, or result in the destruction or adverse modification of habitat of such species which has been determined to be critical. This Public Notice serves as a request to the U. S. Fish and Wildlife Service for any additional information they may have on whether any listed or proposed to be listed endangered or threatened species may be present in the area which would be affected by the activity, pursuant to Section 7(c) of the Endangered Species Act of 1972 (as amended).

7. IMPACT ON CULTURAL RESOURCES: The National Register of Historic Places has been consulted, and it has been determined that there are no properties currently listed on the register which would be directly affected by the proposed work. If we are made aware, as a result of comments received in response to this notice, or by other means, of specific archeological, scientific, prehistorical, or historical sites or structures which might be affected by the proposed work, the District Engineer will immediately take the appropriate action necessary pursuant to the National Historic Preservation Act of 1966 - Public Law 89-665 as amended (including Public Law 96-515).

8. PUBLIC INVOLVEMENT: Any person may request, in writing, within the comment period specified in the paragraph below entitled "RESPONSES," that a public hearing be held to consider this application. The requests for public hearing shall state, with particularity, the reasons for holding a public hearing.

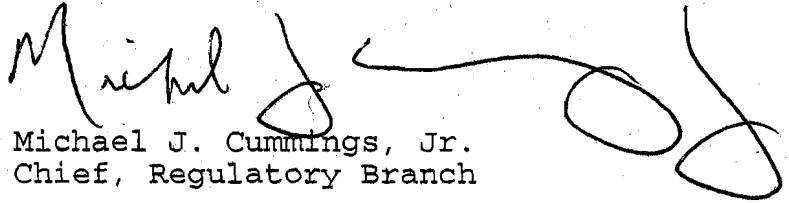
9. EVALUATION: Interested parties are invited to state any objections they may have to the proposed work. The decision whether to issue a permit will be based on an evaluation of the probable impact including cumulative impacts of the proposed activity on the public interest. That decision will reflect the national concern for both protection and utilization of important resources. The benefit which reasonably may be expected to accrue from the proposals must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered including the cumulative effects thereof; among those are conservation, economics, aesthetics, general environmental concerns, wetlands, historic properties, fish and wildlife values, flood hazards, flood plain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food and fiber production, mineral needs, considerations of property ownership and, in general, the needs and welfare of the people. The Corps of Engineers is soliciting comments from the public; Federal, state, and local agencies and officials; Indian Tribes; and other interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the Corps of Engineers to determine whether to issue, modify, condition or deny a permit for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act. Comments are also used to determine the overall public interest of the proposed activity. The evaluation of the impact of the activity on the public interest will include application of the guidelines promulgated by the Administrator, Environmental Protection Agency, under the authority of Section 404(b) of the Clean Water Act (40 CFR Part 230).

10. RESPONSES: A permit will be granted unless its issuance is found to be contrary to the public interest. Written statements concerning the proposed activity should be received in this office on or before the closing date of this Public Notice in order to become a part of the record and to be considered in the final determination. Any objections which are received during this period may be forwarded to the applicant for possible resolution before the determination is made whether to issue or deny the requested DA Permit. All responses to this notice

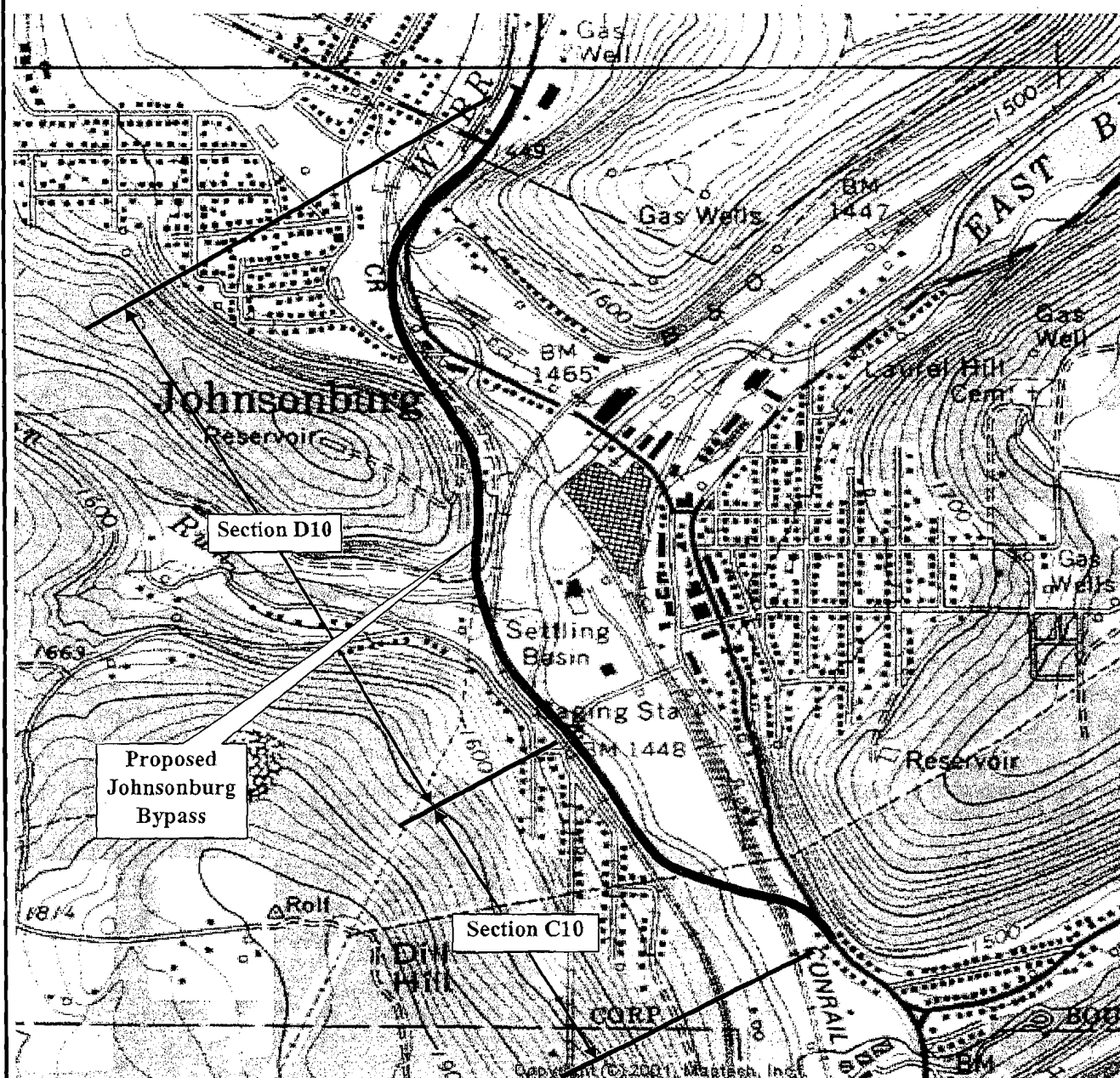
CELRP-OP-F  
Public Notice No. 05-67

should be directed to the Regulatory Branch, attn Nancy Mullen, at the above address, by telephoning (412) 395-7170, or by e-mail at Nancy.J.Mullen@lrp02.usace.army.mil. Please refer to CELRP-OP-F 199701735 in all responses.

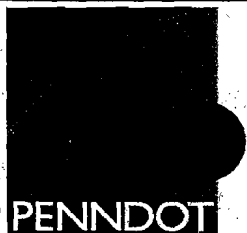
FOR THE DISTRICT ENGINEER:



Michael J. Cummings, Jr.  
Chief, Regulatory Branch



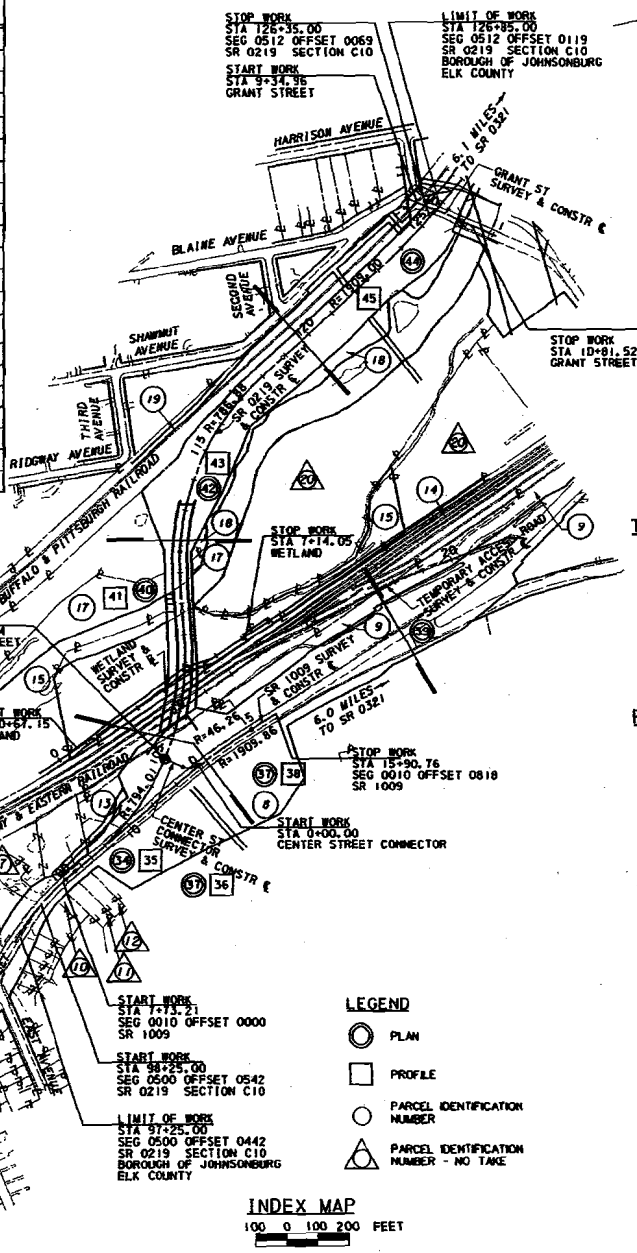
Pennsylvania Department of Transportation, Engineering District 2-0, Clearfield, PA



Elk County, Borough of Johnsonburg  
 S. R. 0219 - Sections C10 and D10, Johnsonburg Bypass  
 Ridgway, PA., U. S. G. S. Quadrangle, 7.5 Minute Series  
 1969 Edition, Photorevised 1981, Photoinspected 1988

# SHEET INDEX BLOCK

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## TABULATION OF SEGMENT EQUALITIES

SR 0219  
 SEGMENT 0050, OFFSET 0492 = STATION 97+75.00  
 SEGMENT 0502, OFFSET 0000 = STATION 100+33.43  
 SEGMENT 0512, OFFSET 0000 = STATION 125+65.63  
 SR 1009  
 SEGMENT 0010, OFFSET 0000 = STATION 7+73.21

## RECORD OF EXISTING TYPES OF ROADWAY PAVEMENT

SR 0219  
 LIMIT OF WORK  
 ADJACENT TO STA 97+25.00  
 24' OF BITUMINOUS SURFACE COURSE, ID-2A, 1 1/2" DEPTH,  
 WEARING COURSE ON BITUMINOUS CONCRETE BASE COURSE,  
 6" DEPTH ON SUBBASE, 17" DEPTH

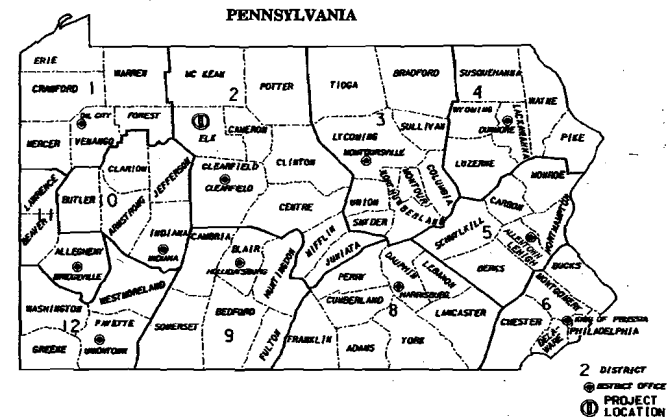
SR 0219  
 STA 97+25.00 TO STA 100+33.43  
 24' OF BITUMINOUS SURFACE COURSE, ID-2A, 1 1/2" DEPTH,  
 WEARING COURSE ON BITUMINOUS CONCRETE BASE COURSE,  
 6" DEPTH, SUBBASE, 17" DEPTH

SR 1009  
 STA 7+73.21 TO STA 15+90.76  
 24' OF BITUMINOUS SURFACE COURSE, ID-2A, 1 1/2" DEPTH,  
 WEARING COURSE, BITUMINOUS CONCRETE BASE COURSE,  
 6" DEPTH, SUBBASE, 17" DEPTH

SR 1009  
 ADJACENT TO STA 15+90.76  
 24' OF BITUMINOUS SURFACE COURSE, ID-2A, 1 1/2" DEPTH,  
 WEARING COURSE, BITUMINOUS CONCRETE BASE COURSE,  
 6" DEPTH, SUBBASE, 17" DEPTH

NOTE:  
 THE DEPTHS OF MATERIAL SHOWN ARE FOR DESIGN PURPOSES ONLY.  
 ANY RISK OF UNANTICIPATED COSTS ASSOCIATED WITH DIFFERENCES  
 BETWEEN THE LISTED DEPTHS AND THE ACTUAL DEPTHS SHALL BE  
 ACCEPTED BY THE CONTRACTOR.

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	2 OF 4
BOROUGH OF JOHNSONBURG				
REVISION NUMBER	REVISIONS	DATE		

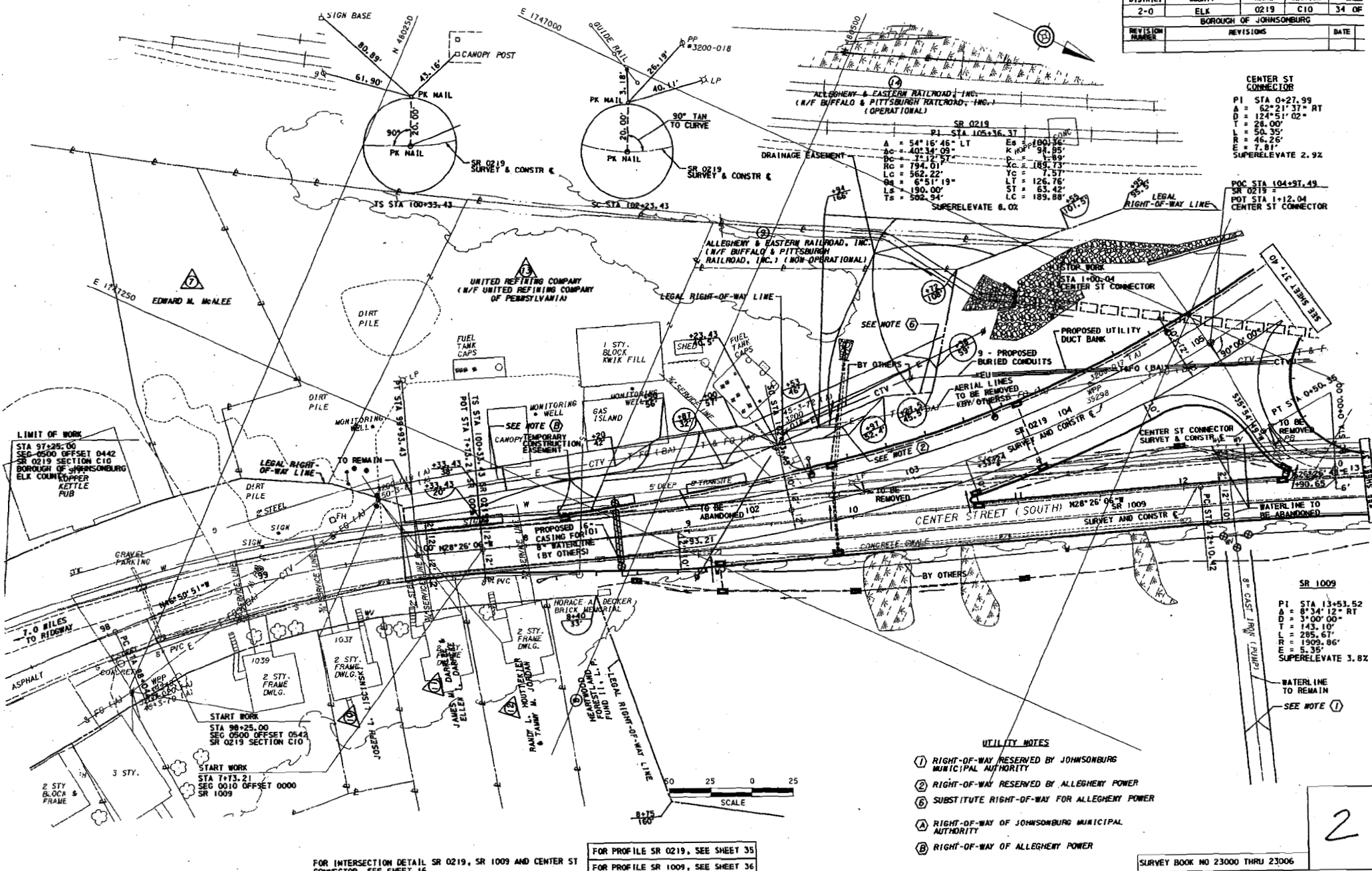


## LIST OF PROPERTY OWNERS

- ① - EDWARD M. MCALICE
- ② - HEARTWOOD FORESTLAND FUND II, L.P.
- ③ - ALLEGHENY & EASTERN RAILROAD, INC. (N/F BUFFALO & PITTSBURGH RAILROAD, INC.) (NON-OPERATIONAL)
- ④ - JOSEPH L. LISINSKI
- ⑤ - JAMES M. DAPPLE & ELLEN I. DAPPLE
- ⑥ - RANDY L. HOUTTEKIER & TAMMY M. JORDAN
- ⑦ - UNITED REFINING COMPANY (N/F UNITED REFINING COMPANY OF PENNSYLVANIA)
- ⑧ - ALLEGHENY & EASTERN RAILROAD, INC. (N/F BUFFALO & PITTSBURGH RAILROAD, INC.) (OPERATIONAL)
- ⑨ - J. M. DETWILER
- ⑩ - HARRY DETWILER
- ⑪ - SAM GUAGLIANONE & JEANNE F. GUAGLIANONE
- ⑫ - ADRIAN REALTY COMPANY (N/F CSX TRANSPORTATION, INC.)
- ⑬ - BUFFALO & PITTSBURGH RAILROAD, INC.
- ⑭ - WILLAMETTE INDUSTRIES, INC. (N/F WEYERHAEUSER COMPANY)

199701735

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	34 OF
BOROUGH OF JOHNSBURG				
REVISION	DATE	BY	REVISIONS	DATE



**CENTER ST CONNECTOR**  
 PI STA 0+27.99  
 A = 62°21'37" RT  
 D = 124°51'02"  
 L = 28.00'  
 E = 50.35'  
 R = 46.26'  
 S = 7.81'  
 SUPERELEVATE 2.9%

POC STA 104+97.49  
 POT STA 1+12.04  
 CENTER ST CONNECTOR

**SR 1009**  
 PI STA 13+53.52  
 A = 8°34'12" RT  
 D = 3°00'00"  
 T = 143.10'  
 L = 285.67'  
 R = 1909.86'  
 E = 5.35'  
 SUPERELEVATE 3.8%

WATERLINE TO REMAIN  
 SEE NOTE (1)

- UTILITY NOTES**
- (1) RIGHT-OF-WAY RESERVED BY JOHNSBURG MUNICIPAL AUTHORITY
  - (2) RIGHT-OF-WAY RESERVED BY ALLEGHENY POWER
  - (6) SUBSTITUTE RIGHT-OF-WAY FOR ALLEGHENY POWER
  - (A) RIGHT-OF-WAY OF JOHNSBURG MUNICIPAL AUTHORITY
  - (B) RIGHT-OF-WAY OF ALLEGHENY POWER

FOR INTERSECTION DETAIL SR 0219, SR 1009 AND CENTER ST CONNECTOR, SEE SHEET 16

FOR PROFILE SR 0219, SEE SHEET 35

FOR PROFILE SR 1009, SEE SHEET 36

SURVEY BOOK NO 23000 THRU 23006

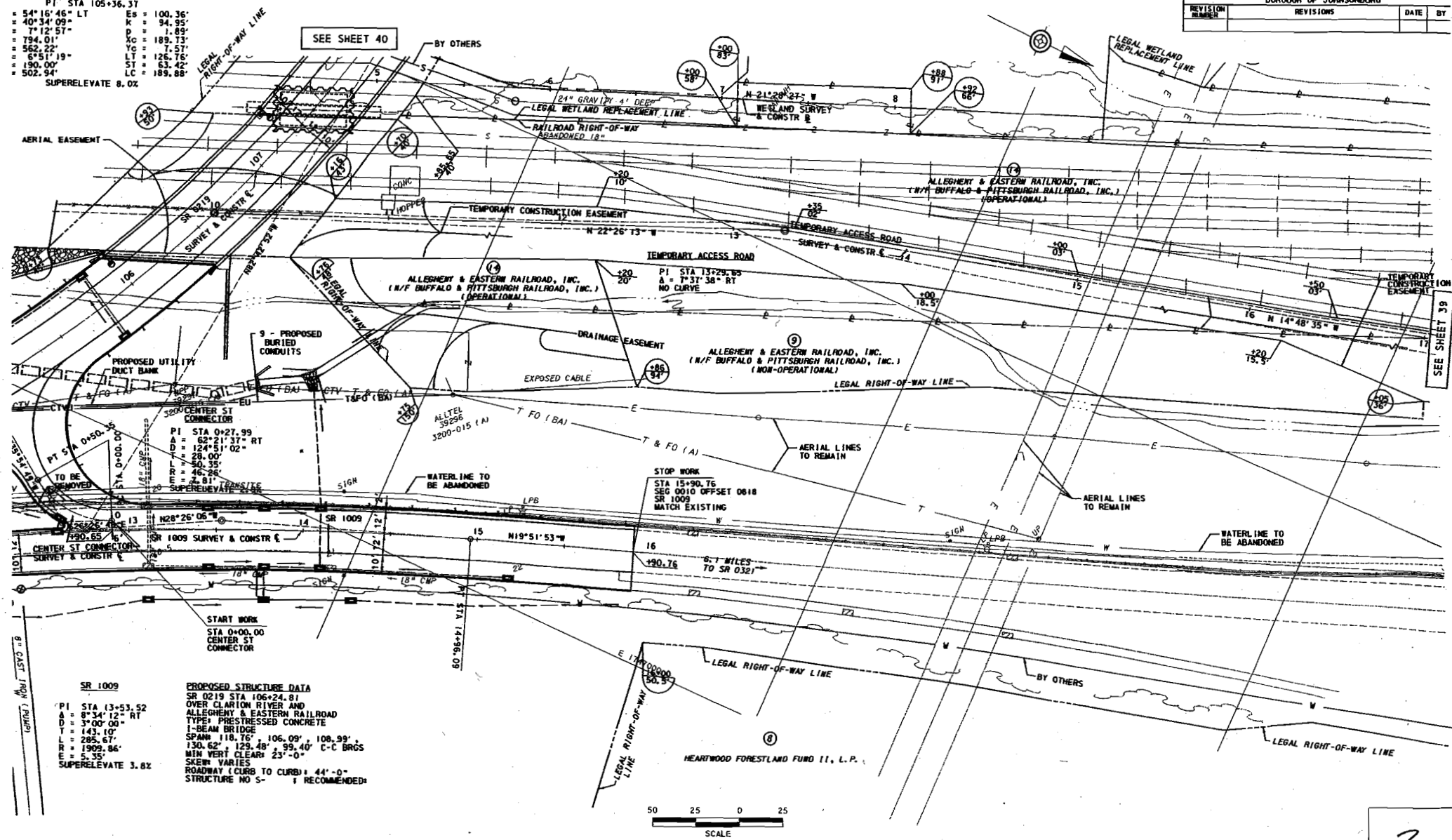
2

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Section C10

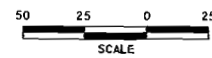
199701735

SR 0219  
PI STA 105+36.37  
= 54°16'46" LT  
= 40°34'09" E  
= 7°12'57" S  
= 794.01'  
= 562.22'  
= 6°51'19" S  
= 190.00'  
= 502.94'  
Es = 100.36'  
K = 94.95'  
D = 1.89'  
Xo = 189.73'  
Yo = 7.57'  
LT = 126.76'  
ST = 63.42'  
LC = 189.88'  
SUPERELEVATE 8.0%



SR 1009  
PI STA 13+53.52  
A = 8°34'12" RT  
D = 3°00'00" RT  
T = 143.10'  
L = 285.67'  
R = 1909.86'  
E = 5.35'  
SUPERELEVATE 3.8%

PROPOSED STRUCTURE DATA  
SR 0219 STA 106+24.81  
OVER CLARION RIVER AND  
ALLEGHENY & EASTERN RAILROAD  
TYPE: PRESTRESSED CONCRETE  
1-BEAM BRIDGE  
SPAN: 118.76', 106.09', 108.99',  
130.62', 129.48', 99.40' C-C BRGS  
MIN VERT CLEAR: 23'-0"  
SKEW VARIES  
ROADWAY (CURB TO CURB): 44'-0"  
STRUCTURE NO 5- I RECOMMENDED:



FOR INTERSECTION DETAIL SR 0219, SR 1009 AND CENTER ST CONNECTOR, SEE SHEET 16

FOR PROFILE SR 0219, SEE SHEET 41  
FOR PROFILE SR 1009, SEE SHEET 38

LIMIT OF AERIAL EASEMENT STA 105+73.00 TO STA 106+93.00 LT  
LIMIT OF AERIAL EASEMENT STA 106+76.00 TO STA 107+70.00 RT

SURVEY BOOK NO 23000 THRU 23006

(37) on sheet 1

Section C10

199701735

3



SR 0219  
PI STA 105+36.37

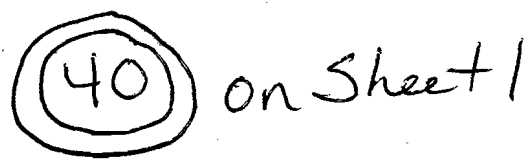
A = 54°16'46"	LT	Es = 100.36'
Ac = 40°34'09"		k = 94.95'
Dc = 7°12'57"		p = 1.89'
Rc = 794.01'		Xc = 189.73'
Lc = 562.22'		Yc = 7.57'
Os = 6°51'19"		LT = 126.76'
Ls = 190.00'		ST = 63.42'
Ts = 502.94'		LC = 189.88'

SUPERELEVATE 8.0%

BM #4518 ELEV 1429.18  
216' RT STA 108+88  
PUNCH MARK ON SAN MANHOLE

<b>HYDRAULIC DATA</b>	
DRAINAGE AREA:	215.8 SQ MI
DESIGN FLOOD:	50 YEAR
DISCHARGE:	9,920 CFS
ELEVATION:	1,430.41
VELOCITY:	5.33 FPS
100-YR FLOOD	
DISCHARGE:	11,300 CFS
ELEVATION:	1,431.04
VELOCITY:	5.47 FPS
FLOOD OF RECORD: JANUARY 1996	
DISCHARGE:	12,800 CFS
ELEVATION:	1,433.20

PROPOSED STRUCTURE DATA  
SR 0219 STA 106+24.81  
OVER CLARION RIVER AND  
ALLEGHENY & EASTERN RAILROAD  
TYPE: PRESTRESSED CONCRETE  
I-BEAM BRIDGE  
SPAN: 118.7', 106.09', 108.99',  
130.62', 129.48', 99.40' C-C BRGS  
MIN VERT CLEAR: 23'-0"  
SKW: VARIES  
ROADWAY (CURB TO CURB): 44'-0"  
STRUCTURE NO S-        ; RECOMMENDED

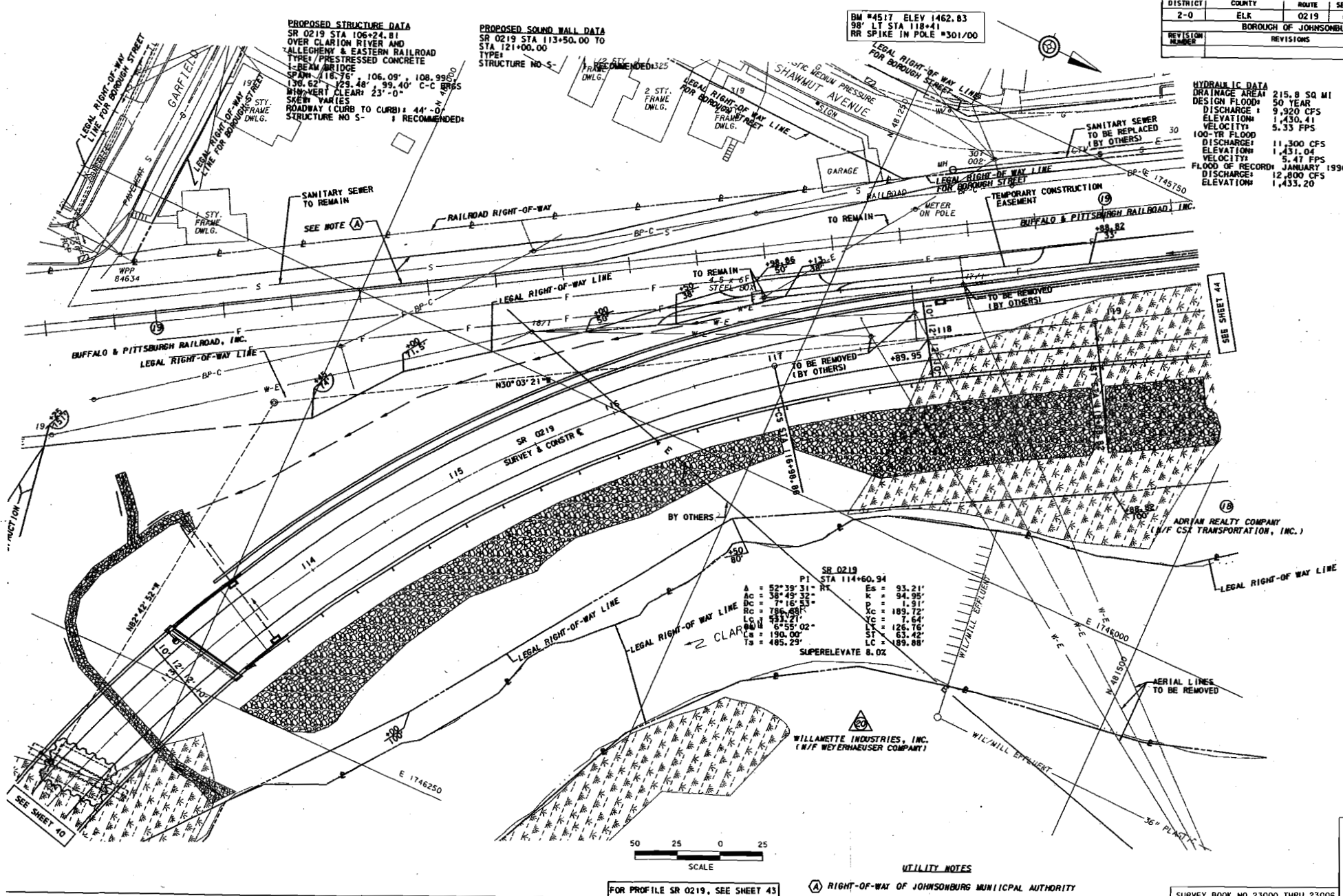


199701735

Section C10

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	42 OF 4
BOROUGH OF JOHNSBURG				
REVISION NUMBER	REVISIONS	DATE	BY	

HYDRAULIC DATA  
 DRAINAGE AREA 215.8 SQ MI  
 DESIGN FLOOD 50 YEAR  
 DISCHARGE 9,920 CFS  
 ELEVATION 1,430.41  
 VELOCITY 5.33 FPS  
 100-YR FLOOD  
 DISCHARGE 11,300 CFS  
 ELEVATION 1,431.04  
 VELOCITY 5.47 FPS  
 FLOOD OF RECORD JANUARY 1936  
 DISCHARGE 12,800 CFS  
 ELEVATION 1,433.20



FOR PROFILE SR 0219, SEE SHEET 43

(A) RIGHT-OF-WAY OF JOHNSBURG MUNICIPAL AUTHORITY

SURVEY BOOK NO 23000 THRU 23006

(42) on Sheet 1

Section C10

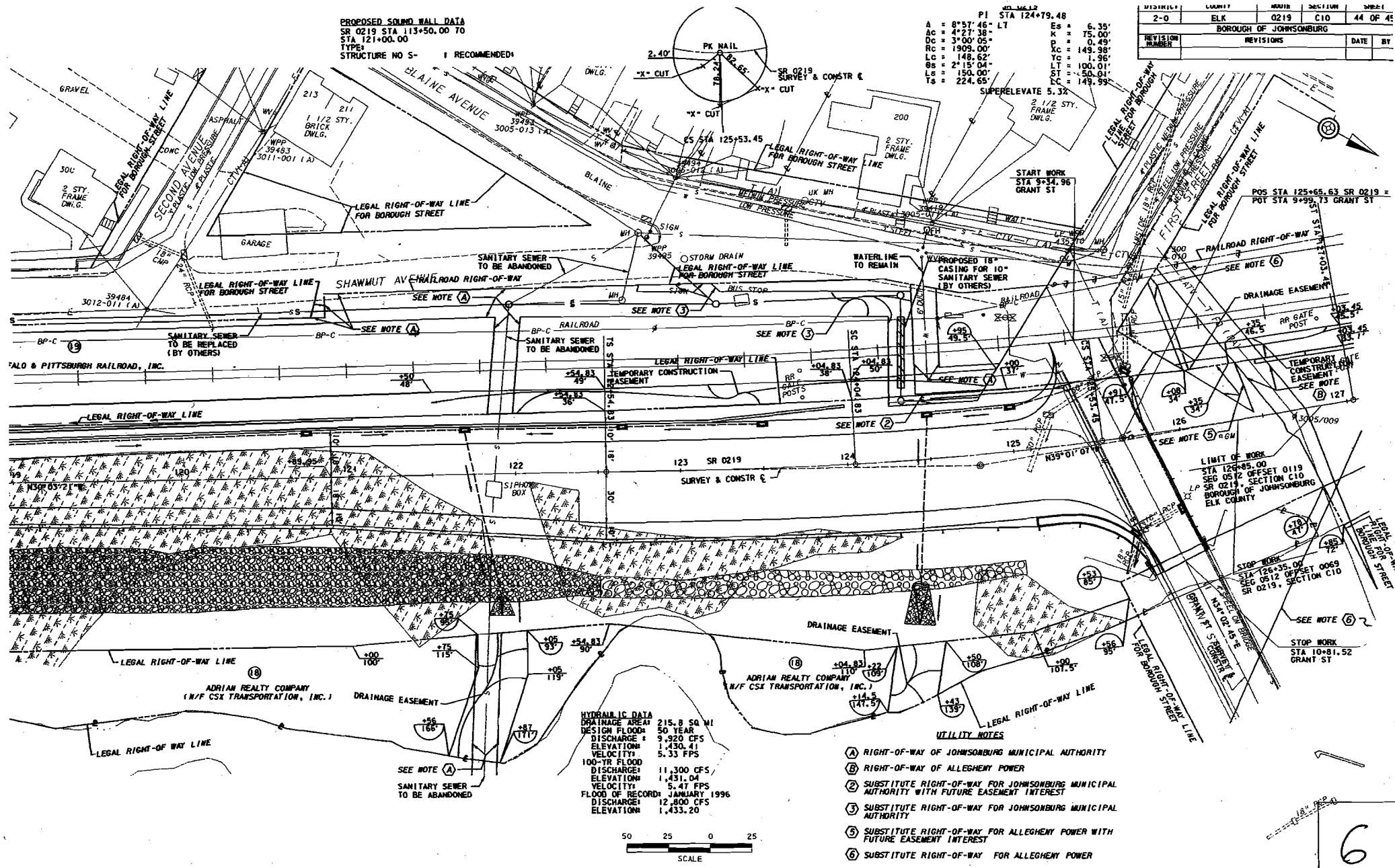
199701735

5

PROPOSED SOUND WALL DATA  
 SR 0219 STA 113+50.00 TO  
 STA 121+00.00  
 TYPE:  
 STRUCTURE NO S- I RECOMMENDED

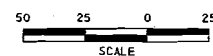
PI STA 124+79.48  
 A = 8°57'45" LT Es = 6.35'  
 Bc = 4°27'38" p = 0.49'  
 Dc = 3°00'05" Kc = 149.98'  
 Lc = 1909.00' Yc = 1.96'  
 Es = 2°15'04" LT = 100.01'  
 Ls = 150.00' ST = 50.01'  
 Ts = 224.65' LC = 149.93'

REVISION NUMBER	REVISIONS	DATE	BY
2-0	ELK 0219 C10 44 OF 45		



HYDRAULIC DATA  
 DRAINAGE AREA 215.8 SQ MI  
 DESIGN FLOOD 50 YEAR  
 DISCHARGE 1 9,320 CFS  
 ELEVATION 1,430.41  
 VELOCITY 5.33 FPS  
 100-YR FLOOD  
 DISCHARGE 11,300 CFS  
 ELEVATION 1,431.04  
 VELOCITY 5.47 FPS  
 FLOOD OF RECORD JANUARY 1996  
 DISCHARGE 12,800 CFS  
 ELEVATION 1,433.20

- UTILITY NOTES
- (A) RIGHT-OF-WAY OF JOHNSBURG MUNICIPAL AUTHORITY
  - (B) RIGHT-OF-WAY OF ALLEGHENY POWER
  - (2) SUBSTITUTE RIGHT-OF-WAY FOR JOHNSBURG MUNICIPAL AUTHORITY WITH FUTURE EASEMENT INTEREST
  - (3) SUBSTITUTE RIGHT-OF-WAY FOR JOHNSBURG MUNICIPAL AUTHORITY
  - (5) SUBSTITUTE RIGHT-OF-WAY FOR ALLEGHENY POWER WITH FUTURE EASEMENT INTEREST
  - (6) SUBSTITUTE RIGHT-OF-WAY FOR ALLEGHENY POWER



FOR INTERSECTION DETAIL SR 0219 AND GRANT ST, SEE SHEET 11

FOR PROFILE SR 0219, SEE SHEET 45

SURVEY BOOK NO 23000 THRU 23006

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Section C10

199701735

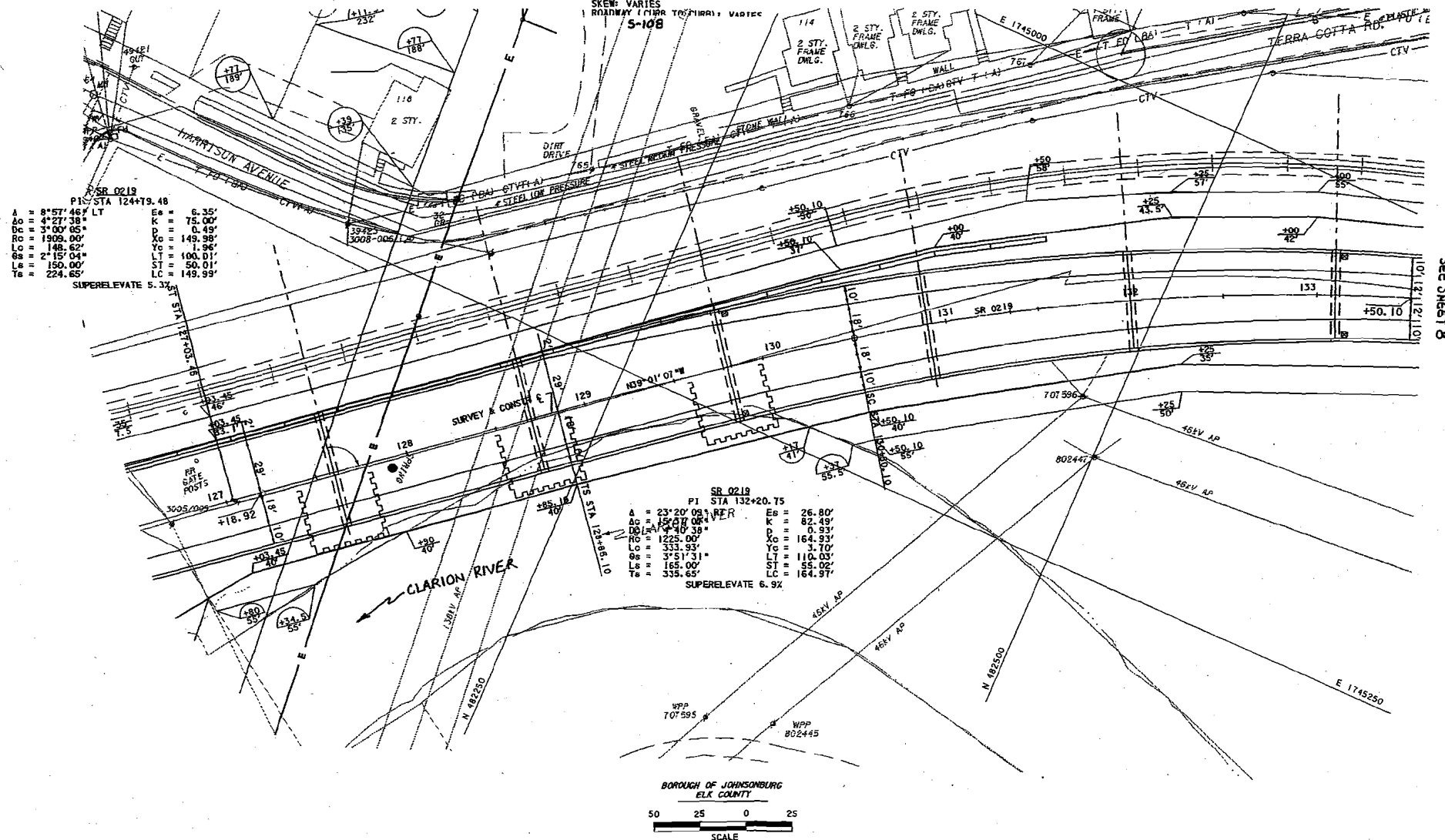
6

DISTRICT	COUNTY	STATE ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	7 OF 13
BOROUGH OF JOHNSONBURG				
REVISION NUMBER	REVISIONS	DATE	BY	

PROPOSED BRIDGE DATA  
 SR 0219 STA 133+10.00  
 OVER JOHNSON RUN AND BUFFALO  
 & PITTSBURGH RAILROAD  
 D: TYPE: COMPOSITE PRESTRESSED  
 CONCRETE 1-BEAM AND STEEL  
 CURVED PLATE GIRDER BRIDGE  
 SPAN: 110'-0", 110'-0", 110'-0"  
 110'-0", 110'-0", 110'-0"  
 190'-0" 280'-0" 190'-0" C-C  
 MIN VERT CLEAR: 24'-6"  
 SKEW: VARIES

ROADWAY (THRU TRAFFIC) VARIET

5-108



SURVEY BOOK NO  
 FOR PROFILE SR 0219, SEE SHEET

7

Section D10

199701735

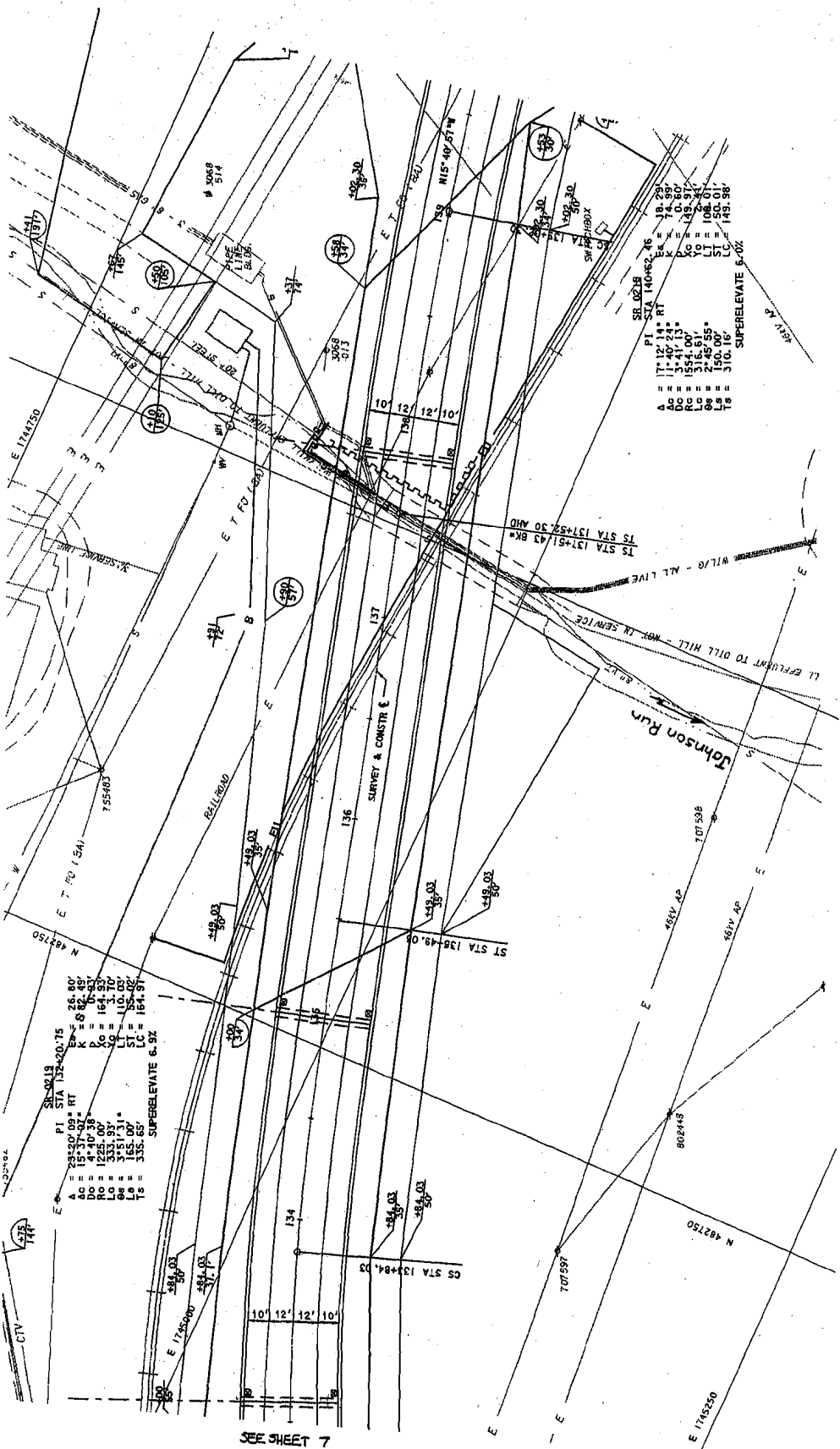
DISTRICT	COUNTY	STATE ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	8 OF 13
DATE	BY	REVISIONS		

8

SURVEY BOOK NO.  
FOR PROFILE SR 0219, SEE SHEET

Section D10

199701735



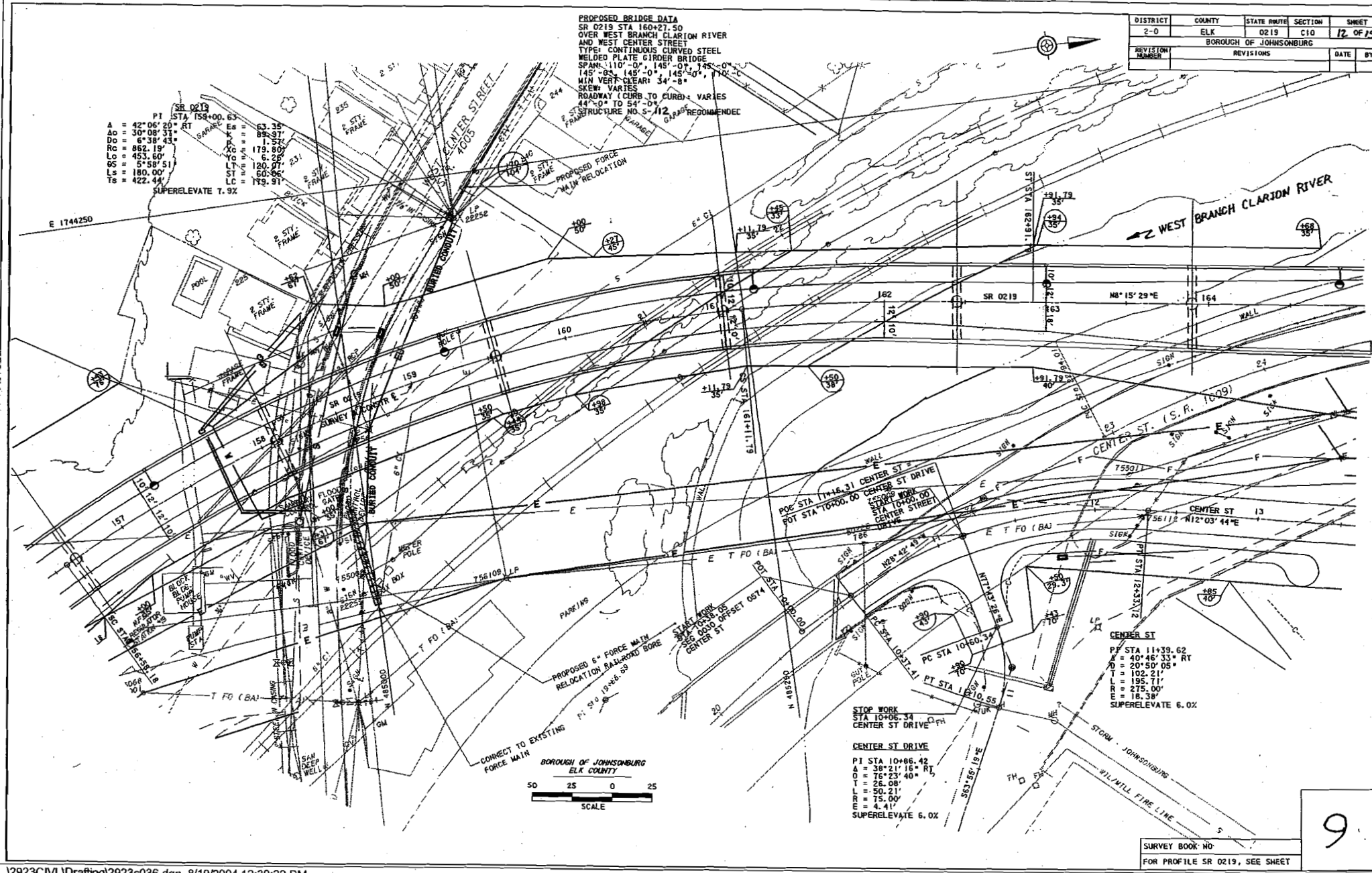
BOROUGH OF JOHNSBURG  
ELK COUNTY

SCALE  
50 25 0 25

DISTRICT	COUNTY	STATE ROUTE	SECTION	SHEET
2-0	ELK	0219	C10	12 OF 15
BOROUGH OF JOHNSBURG				
REVISION NUMBER	REVISIONS			DATE BY

PROPOSED BRIDGE DATA  
 SR 0219 STA 160+27.50  
 OVER WEST BRANCH CLARION RIVER  
 AND WEST CENTER STREET  
 TYPE: CONTINUOUS CURVED STEEL  
 WELDED PLATE GIRDER BRIDGE  
 SPAN: 110'-0", 145'-0", 145'-0"  
 145'-0", 145'-0", 145'-0", 110'-0"  
 MIN VERT CLEAR: 34'-8"  
 SKEW: VARIES  
 ROADWAY (CURB TO CURB): VARIES  
 44'-0" TO 54'-0"  
 STRUCTURE NO. 5-112 RECOMMENDED

SR 0219  
 STA 158+00.63  
 A=42°06'20" RT  
 Do=30°08'31"  
 R=6°38'43"  
 L=862.19'  
 Lo=453.60'  
 Os=5°58'51"  
 Ls=180.00'  
 Ts=422.44'  
 SUPERELEVATE 7.9%



STOP WORK  
 STA 10+06.34  
 CENTER ST DRIVE  
 CENTER ST DRIVE  
 PI STA 10+66.42  
 A = 38°21'18" RT  
 D = 75°23'40"  
 L = 28.08'  
 T = 50.21'  
 R = 75.00'  
 E = 4.41'  
 SUPERELEVATE 6.0%

CENTER ST  
 PI STA 11+39.62  
 A = 40°46'33" RT  
 D = 20°50'05"  
 L = 102.21'  
 T = 195.11'  
 R = 275.00'  
 E = 18.38'  
 SUPERELEVATE 6.0%

SURVEY BOOK NO.  
 FOR PROFILE SR 0219, SEE SHEET

9

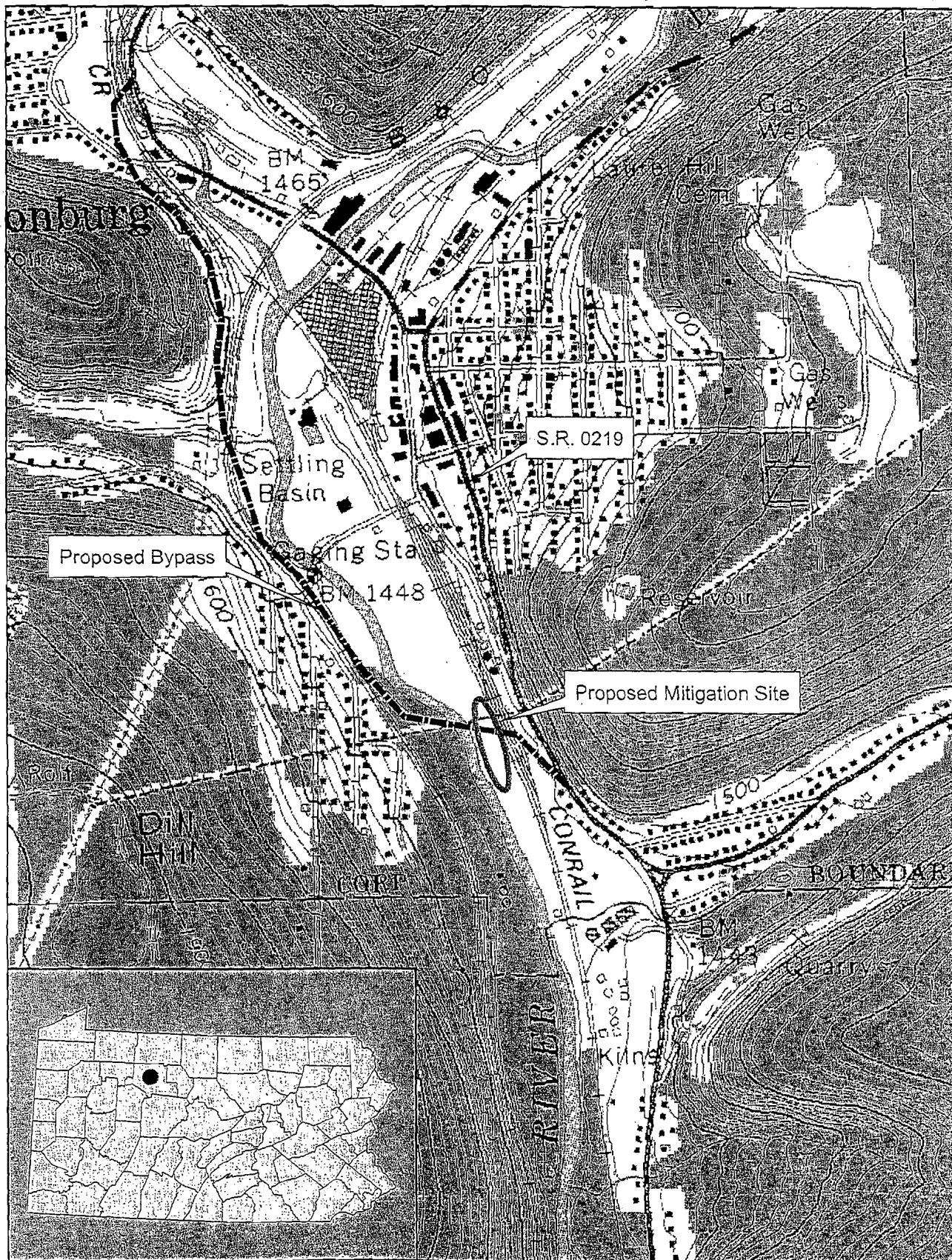
Section D10

199701735



# Figure 1 Proposed Mitigation Site Location

S.R. 0219, Section C10  
Borough of Johnsonburg, Elk County, Pennsylvania



Map Source: USGS Topographic Quadrangle; Ridgway, PA

600 0 600 Feet

